

Heathrow Airside Drivers Handbook

Version 2 (12/05/2024)

Heathrow



Introduction

This handbook is aimed at employees who possess an Airside Driving Permit (ADP). Its goal is to outline the expectations for ADP holders and to ensure their safety when driving across the airside road network.



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Fitness to drive

You are required to be in fit condition for all work tasks, including driving responsibilities. If you are unable to perform your duties, it is your responsibility to notify your manager as soon as possible.

Your ability to drive safely is significantly influenced by your physical health, psychological and emotional wellbeing, as well as your overall attitude toward driving. Therefore, it is important to disclose any health issues or personal circumstances that may impact your ability to drive to your employer without delay.

Factors which can negatively affect your fitness to drive include:

- **Fatigue**
- **Consumption of alcohol**
- **Use of illegal drugs**
- **Certain prescription and over-the-counter medications**
- **Mental health issues**
- **Temporary illness**
- **Underlying medical conditions**

Daily stress events

These are stressors which are generated by day-to-day events, such as work-related issues, relationship difficulties, financial and family matters.

It is important to manage these stress events in order to minimise their impact on your driving. Contact your line manager to discuss any stress events that could have a negative impact on your driving.

It is an offence to drive under the influence of drugs or alcohol airside – you could receive up to 20 points and lose your airside license.

Airside Driver Permit (ADP)

All drivers intending to drive vehicles airside, require an ADP (Airside Driver Permit) license.

The type of permit issued to drivers depends on their specific role. To obtain a driving permit, drivers must have completed a relevant training course and assessment, which includes a practical component.



Types of Permits:

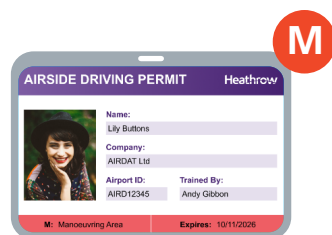
'A' Airside Driving Permit

The **'A' ADP** is the initial permit which allows a driver to operate on airside roads and aprons, including uncontrolled taxiway crossings. The holder must not cross the double white lines into the manoeuvring area.

This permit is valid up to a maximum of three years, and before it can be re-issued, appropriate refresher training and testing of the driver's competency must be completed and recorded.

Requirements:

- Full DVLA issue driving licence check.
- Medical completed at least once every 3 years.
- Refresher training every 3 years.



'M' Airside Driving Permit

The **'M' ADP** allows a driver to operate on the airside roads and aprons, and the manoeuvring area, but excludes the runways. An 'M' ADP is required for any operation which is required beyond the double white lines.

The 'M' permit is valid for three years, and can only be re-issued once refresher training and a competency test has been successfully completed and recorded.

Requirements:

- Full DVLA issue driving licence check.
- Medical completed at least once every 3 years.
- Refresher training every 3 years.
- Drivers must have held an **'A' ADP for at least 6 months**.

The unique Airside Driver Number (ADP number) that is issued to an individual will be associated with that individual throughout their airside driving career at Heathrow and the driver will not be issued with a second driver identity even if they join a new employer. This will assist in tracking driver history at Heathrow.



'R' Airside Driving Permit

The **'R' ADP** provides access to airside roads, aprons, the manoeuvring area and runways, and will require successful completion of an enhanced training package and assessments.

The 'R' permit is valid for one year, and will only be re-issued once refresher training and a competency test has been successfully completed and recorded.

Requirements:

- Full DVLA issue driving licence check.
- Medical completed at least once every 3 years.
- Refresher Training every year.
- Drivers must have held and **'M' ADP for at least 12 months**.

The ADP holder is responsible for maintaining a current license and carrying it at all times. Please ensure you notify the relevant persons within your company well in advance if your license is expiring.

Safety Six

The Safety Six safety campaign consists of six essential safety measures to ensure the safety of everyone while working airside.



Always wear your seatbelt

Ensure all passengers in your vehicle are wearing a seatbelt.
Always challenge those who aren't wearing theirs.



Correct use of PPE

Wear appropriate PPE for the task.
Always wear PPE, as instructed by your company.



No distractions when driving

Do not use handheld devices while driving.
Pull over if you need to use a handheld device.



FOD Housekeeping

Ensure you dispose of any FOD you come across.
Return all stand equipment & GSE to its appropriate parking areas.
Ensure vehicles are parked safely & correctly.



Check your vehicle / dolly

Check vehicle lights are working.
Check tyre tread depth and that there are no bulges or cracks in tyres.
Ensure the obstruction light is working.
Check for damage to the vehicle.
Check the vehicle apron pass is valid.
Ensure you report any defected vehicles to your company.
Functional checks for GSE.



Safety reporting

Ensure you report all reportable incidences through the correct channels.
Ensure you report all close calls / near misses you come across.

Safety Six



BUCKLE UP
NO MATTER HOW SHORT YOUR JOURNEY IS!

Always wear your seatbelt when driving airside.
Ensure your passengers wear it too, and challenge those who don't.

Team Heathrow



1. Always wear your seatbelt

- Wear the appropriate PPE for the task and as instructed by your company.
- Care for, clean and maintain your PPE and inform your supervisor if repair or replacement is necessary.
- Hi-Vis clothing must always be worn in all baggage and external airside areas and be fully fastened.
- Protective footwear must be worn across baggage, apron, and manoeuvring areas at all times.
- Ear protection must be worn when working on operational aircraft stands.

CORRECT USE OF PPE
Hi Vis clothing must always be worn in all baggage and external airside areas and be fully fastened.

Protective Footwear must be worn in all baggage, apron and manoeuvring areas at all times.

Team Heathrow



2. Correct use of PPE

- Always wear your seatbelt.
- Ensure your passengers wear it too.
- Challenge those who do not.

Failure to wear a seatbelt while driving is classified as an airside driving offense and results in a deduction of 6 points from your ADP permit. Similarly, failure to ensure passengers wear seatbelts constitutes an airside offense and incurs a deduction of 1 point from your ADP permit.

Safety Six



PHONES DOWN
EYES UP
NO DISTRACTIONS WHEN DRIVING

Do not use any electronic device or hold any items that may cause distraction while driving.
These include in-ear devices, such as air pods.

THE SAFETY SIX

Team Heathrow

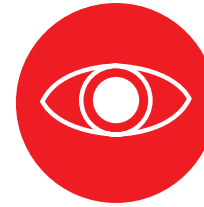


3. No distractions when driving

- Do not use any electronic or handling device that may cause distraction while driving, such as in-ear devices (e.g. airpods), mobile phones, sat navs, tablets, or any device that can send or receive data.
- Pull over in a safe space if you need to answer a call.

Distractions

We would usually put distracted driving into one of three categories:



Visual

Visual or audible distractions are things that can take your eyes or attention off the road (i.e. looking at GPS, conversations etc.).



Manual

Manual distractions are things that cause you to take your hands off the wheel or your feet off the pedals (i.e. searching in bag / glove box, eating / drinking, grooming etc.).



Cognitive

Cognitive distractions are things that you are thinking about or feeling that cause you to take your mind off the task of driving (i.e. fatigue, personal issues etc.).

Pre-trip:

- Make any adjustments to mirrors, temperature, seat etc. before driving.
- Ensure that any loose objects are secured. These can cause a distraction while driving.
- Ensure that you are well rested and in the right frame of mind to be behind the wheel.
- Give yourself ample time to get where you are going.

While driving:

- Drivers must not use a hand-held mobile phone whilst the vehicle is in motion.
- Audio devices of any kind (such as headphones / air pods) can detract from your ability to drive safely and cause distraction.
- Keep noise to a minimum. Noises, such as loud music, can be a distraction.
- Keep your eyes on the road ahead.
- Obey the rules of the road.

Driving while using handheld devices is considered an airside offence and results in a deduction of 6 points on your ADP permit.

Safety Six



PICK IT UP

FOD IS EVERYONE'S RESPONSIBILITY

Dispose of any Foreign Object Debris you come across in the dedicated FOD bins.

Ensure that chocks, cones, FEGP and PCA are stored properly after each use.

Make sure all vehicles and GSE are parked correctly and kept FOD-free.

Call Airside Ops at 0208 745 6024 for any FOD management requests.

FOD

4. Housekeeping

- Dispose of any FOD you come across by using the bins provided at head of each stand.
- Return all stand equipment and GSE to their appropriate parking areas.
- Ensure vehicles are parked safely and correctly, and left FOD free.

INSPECT IT

ENSURE YOUR EQUIPMENT IS SAFE TO USE

Inspect your vehicle and equipment before each use.

Report and red-tag any faulty equipment.

Ensure your load is fully secured and stops on dollies are correctly engaged before driving off.

Checklist

5. Check your vehicle

- Conduct a vehicle check before each use.
- Check lights, obstruction light, tyres, and seatbelt.
- Do a brake check.
- Report any defects to your company and red-tag faulty equipment.
- For trailers transporting ULDs, ensure rear reflectors are serviceable and locking mechanism correctly working.
- Do not operate red-tagged equipment.

Call Airside Ops at **0208 745 6024** for any FOD management requests.

Scan the QR code and watch the video demonstrating a recommended vehicle pre-use inspection exercise

Safety Six



6. Safety reporting

- Ensure you report all incidents through the correct channels.
- In case of an emergency, call 222 (from any internal line) or 0208 759 1212 (from any mobile phone). Remember, the emergency phone number is on the back of your ID pass.
- In the event of a road traffic collision or dropped loads incident, please do not move the equipment from the scene until the emergency services arrive on-site.
- Report any close calls / near misses you witness through the Airport Community App.

Ensure you report all reportable incidents through the correct channels as soon as possible.

In case of an emergency, dial 222 from any internal line or 020 8759 1212 from any mobile phone.

In the event of an airside incident, do not move the equipment from its position until authorised to do so.



Scan here to view all safety posters



How to report a Close Call via the Airport Community App. It's a simple 3 steps to report a Close Call.

- Step 1:** Download the App.
- Step 2:** Open 'Close Call Reporting'.
- Step 3:** Fill out details of the Close Call and submit.



How / What to Report

Dropped ULDs	In Baggage Hall	07711 146 323
	Anywhere Else	222 02087 591 212
Road Traffic Collision	222	02087 591 212
Infrastructure Damage	222	02087 591 212
Vehicle Breakdowns	656 024	02087 456 024
Personal Injury	222	02087 591 212
Engineering Help Centre (faults and building cleaning requests)	666 555	02089 766 555

Key Contacts

Baggage: 07711 146 323
Airside Ops: 02087 456 024 (Internal 656 024)
Airport Emergency / APOC Control: 02087 591 212 (Internal 222)

Speed Limits

5	10	20	30
Stands, Inter-stand Clearways and Baggage Areas	Certain Roads	Roads and Cargo Tunnel	ART, Link Tunnels and Southern Perimeter Road



- While reporting an emergency airside, please ensure you ask for the appropriate service, such as Police, AFRS or Ambulance, and provide the following information as a minimum:

1. Location
2. Nature of the occurrence with as much detail as possible
3. Personnel or equipment involved
4. Contact telephone number

Airfield road speed limit map

5

Stands, Inter-stand Clearways and Baggage Areas

10

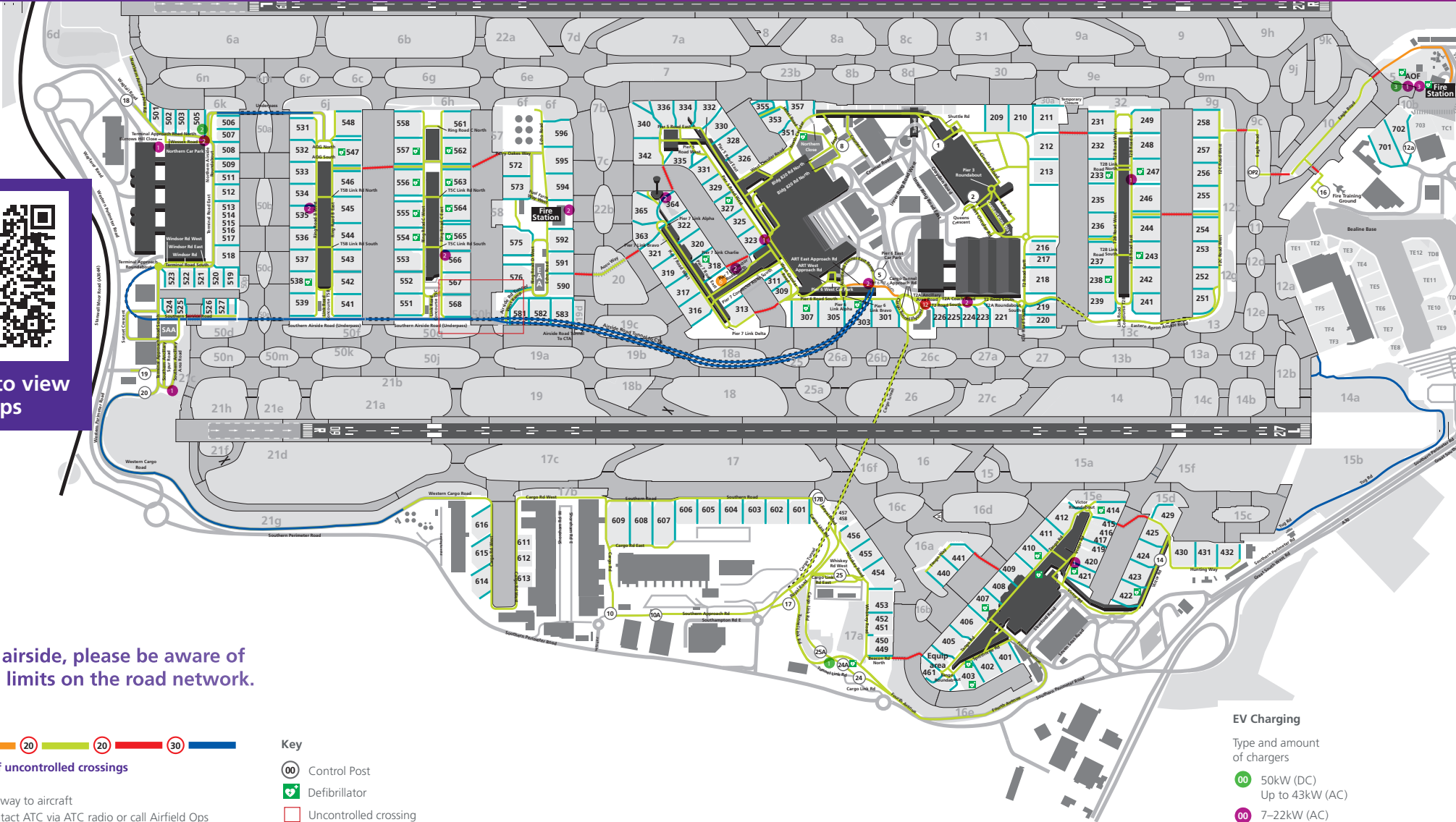
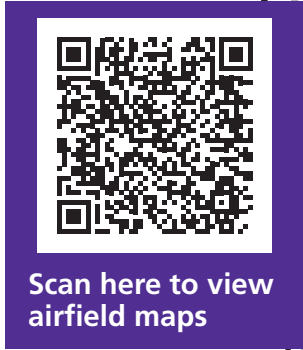
Certain Roads

20

Roads and Cargo Tunnel

30

ART, Link Tunnels and Southern Perimeter Road



When driving airside, please be aware of varying speed limits on the road network.

Speed restriction



Top tips for safe use of uncontrolled crossings

- Speed limit is 20MPH
- Always stop and give way to aircraft
- If you breakdown contact ATC via ATC radio or call Airfield Ops on 0208 745 6459
- If you can't see the other side then don't cross

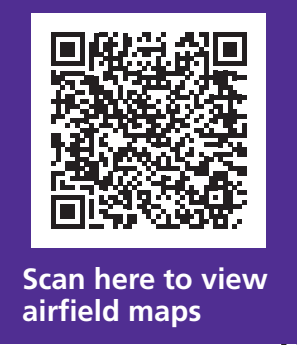
Key

- Control Post
- Defibrillator
- Uncontrolled crossing for restricted vehicles only
- Uncontrolled Taxiway crossing
- Tunnel

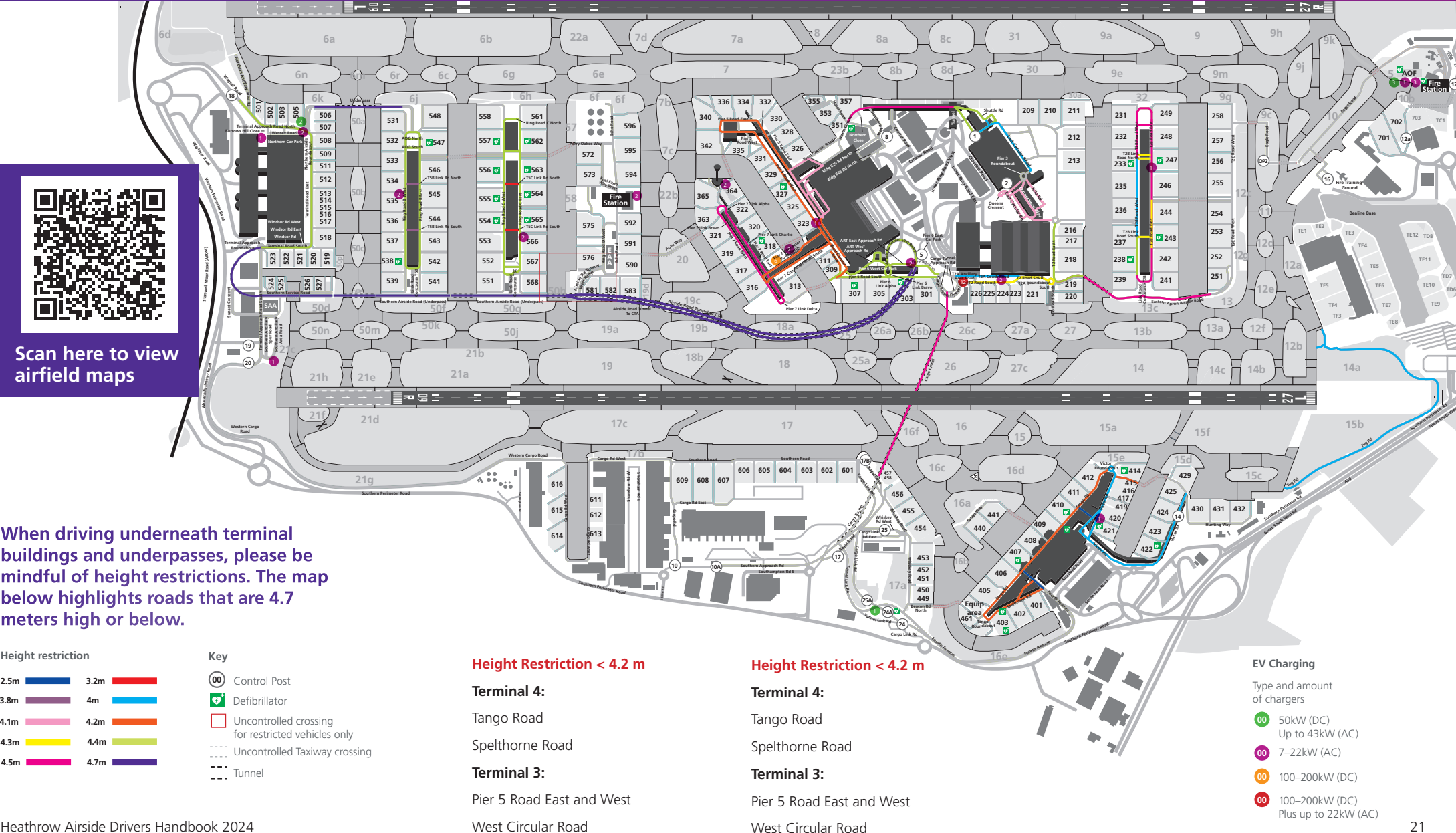
EV Charging

- Type and amount of chargers
- 50kW (DC)
 - Up to 43kW (AC)
 - 7-22kW (AC)
 - 100-200kW (DC)
 - 100-200kW (DC) Plus up to 22kW (AC)

Airfield road height restriction map



When driving underneath terminal buildings and underpasses, please be mindful of height restrictions. The map below highlights roads that are 4.7 meters high or below.



Airside Tunnels

At Heathrow, there are two main airside tunnels that link different terminals:

- The ART (Airside Road Tunnel) Tunnel, which connects the Central Terminal areas (Terminal 1, 2 and 3) to Terminal 5.
- The Cargo Tunnel, which links Terminal 4 with the CTA.



- A** ART Tunnel
- B** Cargo Tunnel

Drivers must not:

- Overtake in Airside Tunnels except in an emergency.
- Attempt to turn or reverse in the tunnel or on its approaches.
- Stop in the tunnel or on its approaches unless the way ahead is blocked by other vehicles or it appears unsafe to proceed.

Vehicle Restrictions in Airside Tunnels:

- Maximum permitted height is **4.5m**.
- Maximum permitted length is **18.75m**.
- Maximum permitted width is **2.55m**.

ART Tunnel

Speed

Maximum 30 mph.

Beacons

Switch off obstruction lights when entering the tunnel.

Red X

If a Red X is shown above your lane, **DO NOT** enter the tunnel.

In the event of a prolonged 'Loss of ART' event, vehicles must use an alternative airside route via the uncontrolled crossing network.



Cargo Tunnel restriction

Speed

Maximum 20 mph. An average speed check is in place, and you could face penalty points if you are caught speeding within the tunnel.

Distance

Maintain **3 'Dots'** between own vehicle and any vehicle in front whilst in motion.

Maintain **9 'Dots'** between a coach and any vehicle in front, including when stationary.

Beacons

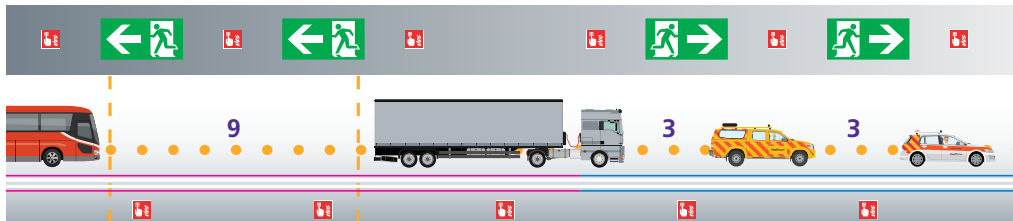
Switch off obstruction lights when entering the tunnel.

Red X

If a Red X is shown above your lane, **DO NOT** enter the tunnel.



The below diagram demonstrates the correct spacing required:



Cargo Tunnel contingency



In the event of a prolonged 'Loss of Cargo Tunnel' event, a contingency diversion of vehicle traffic is implemented across the manoeuvring area (under Leader Escort).

When the Cargo Tunnel is closed, drivers should present themselves at the appropriate muster point, which will usually be adjacent to Stand 254 or 432 and await an escort vehicle to take them across the manoeuvring area.

- Diversion signs will be in place to show the alternative routes.
- Traffic Marshals will be in place at the appropriate points across the route.

There will be 2 routes available:

1. From T4 to the Central Terminal Area (via the Eastern Tug Road)

- Go to the head to stand 432 and wait for a leader vehicle to escort you over.

- Ensure you wait within the clearway, where the sign is indicating for you to wait.

2. From the Central Terminal Area to T4 (via the Western Tug Road)

- Go to head of stand 254, where you will find a road holding area. Wait for a leader vehicle to escort you over.
- Ensure you wait where the sign is indicating for you to wait.

Important!

- Ensure that all vehicle loads are secure before setting off.
- Ensure that vehicle obstruction lights are fitted and switched on.
- Follow all signs and markings along the diversion route – including those along the tug road itself. A large, hatched area is painted on the tug road that demarcates an area in which vehicles must not stop.
- **DO NOT** enter the manoeuvring area without a leader escort or without an instruction from a Heathrow representative.
- Drivers must await instructions from the leader vehicle and **DO NOT** enter the manoeuvring area if unsure.
- Have all windows closed whilst travelling across the manoeuvring area under leader escort.
- Follow the leader vehicle during the transit across the taxiway system, maintaining a safe distance behind the vehicle in front.
- Immediately stop the vehicle and hold position in the event of becoming detached from the convoy or disorientated. **Drivers must not attempt to self-maneuvre back to the convoy or vehicle holding point.**

No overtaking

Overtaking is strictly prohibited when driving airside unless in an emergency or instructed to do so by police, airside operations or an airport official.



Inter-stand clearway congestion



The inter-stand clearway is an area between stands which can be used to enter the stand during the turnaround and gives access to the aircraft for emergency services. **Drivers are reminded that parking in the clearway is strictly prohibited and could result in penalty points being awarded.**

If the aircraft on either side of the clearway are parked on the associated centrelines, vehicle operators can be assured that there is no risk of coming into contact with the wing of the aircraft, as long as the vehicle remains within the confines of the inter-stand clearway.

Vehicles operating on an aircraft stand or inter-stand clearway must have an amber obstruction light illuminated. (OSI 005 – 8.1.2, 8.2.1 & 8.2.2)

Driving in adverse weather conditions

Driving in adverse weather conditions can significantly impact on road safety. Here are some tips to stay safe while driving in different weather scenarios:

Strong winds:

- Keep a low speed and grip the steering wheel with both hands.
- Prepare for strong winds and gusts on exposed stretches of road.
- Maintain a safe distance from other vehicles.
- Be aware of flying debris.
- Exercise care when opening vehicle doors.

Heavy rain and floods:

- Leave twice as much space from the vehicle in front.
- If steering feels light due to aquaplaning, ease off the accelerator and slow down gradually.
- Avoid driving into flood water that is moving or more than 10cm (4 inches) deep.
- After driving in wet conditions, test brakes as soon as possible.

Fog:

- Maintain a greater distance from the vehicle in front, increase the gap to at least 4 seconds.
- Keep your windscreen clear and demisted.
- Don't rely on following the tail lights of the vehicle in front, as these can give a false sense of security.

Snow and ice:

- Keep your vehicle well-ventilated. The car heater on full can quickly make you drowsy.
- Keep your speed down and give yourself time! Always reduce your speed smoothly and in plenty of time on slippery surfaces.
- Avoid harsh braking and acceleration, or sharp steering. Slow down in plenty of time before bends and corners.
- Expect the unexpected. Try to anticipate the actions of other road users.



Low visibility operations

Low Visibility Safeguarding

This is the process carried out by Heathrow at the request of ATC, which prepares the airfield for Low Visibility Procedures. This results in the introduction of restricted access to the manoeuvring area and the protection of the Instrument Landing System (ILS), Localiser Sensitive Area (LSA) and Glidepath Sensitive Area (GSA).

Low Visibility Procedures

These are the actions that are carried out by ATC with respect to aircraft operations. This results in further restrictions on access to the manoeuvring area, the protection of the ILS, LSA and GSA, and a reduced aircraft movement rate.

- When Low Visibility Safeguarding is in force, 'free-ranging' is suspended with the exception of the following: Heathrow Airside Operations, Heathrow AFRS and Airline / Ground Handlers Aircraft Tugs.
- All other vehicles must vacate the manoeuvring area unless carrying out essential operational tasks and are under direct ATC control.
- Uncontrolled crossings must not be used in visibility of less than 150 metres. As a general guide, a driver must not use an uncontrolled crossing if, when stationary at the double white lines, the buildings beyond the other side of the crossing cannot be seen clearly.

Top tips for driving in fog:

- Maintain greater separation distance between your vehicle and the vehicle in front.
- Consider the use of fog lights.
- Open your window to listen for traffic at junctions if you can't see.

What you shouldn't do when driving in fog:

- Don't rely on automatic lights – they might not come on in fog.
- Don't rely on daytime running lights alone – they only light the front.
- Don't tail another car's rear lights – it can give a false sense of security and it's dangerous.
- Don't speed up to get away from a vehicle that's too close behind you.



Strong wind conditions

Strong wind conditions can have a significant impact on drivers and aircraft safety in general.

For example, strong crosswinds can compromise vehicles stability and control, particularly affecting steering. In open areas such as bridges or exposed roadways, vehicles may sway or drift due to the force of strong winds. Visibility can also be reduced as dust and debris may be blown onto the road.

In addition to these challenges, strong winds can result in unsecured objects to be moved in an uncontrolled manner, posing risks of significant damage to aircrafts on the manoeuvring area or stand, vehicles, and infrastructure. This also increases the potential for injuries to pedestrians. Finally, loose items during strong winds pose a serious risk of engine ingestion, particularly during critical phases of flight such as take-off and landing, making their impact on aircrafts highly significant.

At Heathrow, a weather warning for strong wind conditions will be issued by the Met Office for wind speeds of 20kts and / or gusts of 28kts. Gale warning will be issued for wind speed of 34kts and / or gusts of 43kts.

When a strong wind warning has been issued, or when strong wind conditions are experienced, please ensure the following actions are taken:

Prevent the accumulation of FOD:

- Ensure good housekeeping of vehicles and remove any loose items.
- Use the dedicated FOD bins at the head of each stand to dispose of any FOD found.
- Ensure waste bins are properly closed after each use.

Exercise extra care when driving:

- Keep a low speed and grip the steering wheel with both hands.
- Prepare for strong winds and gusts on exposed stretches of road.
- Maintain a safe distance from other vehicles.
- Be aware of flying debris.
- Be cautious when opening vehicle doors.

Ensure ULDs are adequately secured, and doors / curtains on ULDs and baggage trailers are properly closed.

- Ensure parking brakes are set on all vehicles and GSE, including dollies and baggage carts.
- Ensure stabilisers on GSE are correctly deployed.
- When not in use, ensure passenger steps are fully lowered, and stabilisers deployed.
- Ensure locks on FEGP units are applied during and after use.
- Ensure aircrafts are chocked in accordance with your company's strong wind policy.
- Ensure safety cones are removed in accordance with your company's strong wind policy.

Useful Contacts:

- Call **0208 745 0802 (Airside Operations)** if you observe any obstructions, loose materials, or equipment moving into the wind uncontrolled. If safe, and possible to do so, please also take action to secure it.
- Call **07514 938 343 (Airside Compliance Team)** if there is a requirement for ULDs to be left on the ground.



Vehicle Collisions Hotspots: Terminal 2

(Jan 2023-Jan 2024)

These are the hotspot areas for road traffic collisions at Terminal 2. Please ensure you're vigilant at all times.



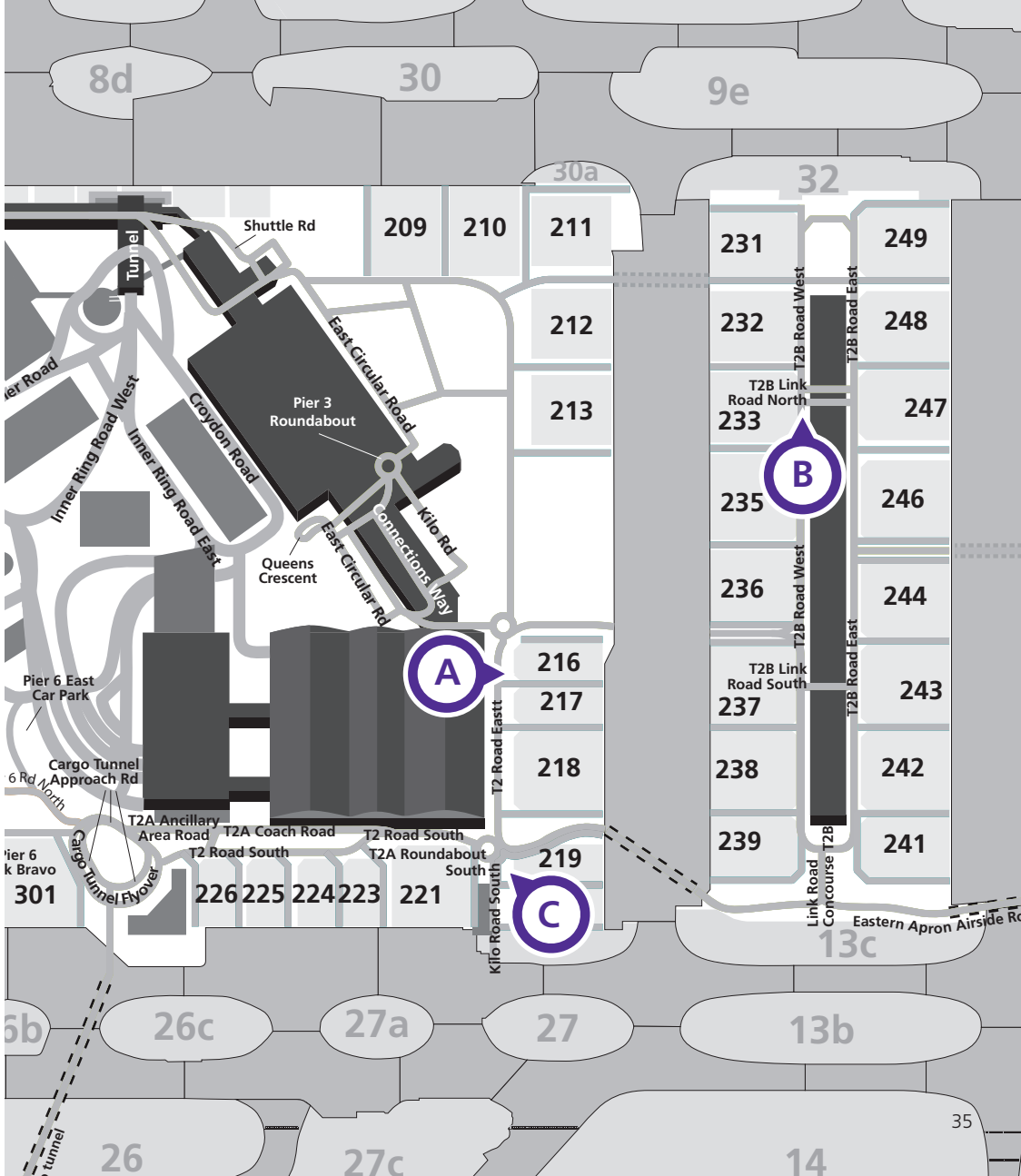
A T2 Road East



B T2B Road West



C T2A Roundabout South



Vehicle Collisions Hotspots: Terminal 3

(Jan 2023-Jan 2024)

These are the hotspot areas for road traffic collisions at Terminal 3. Please ensure you're vigilant at all times.



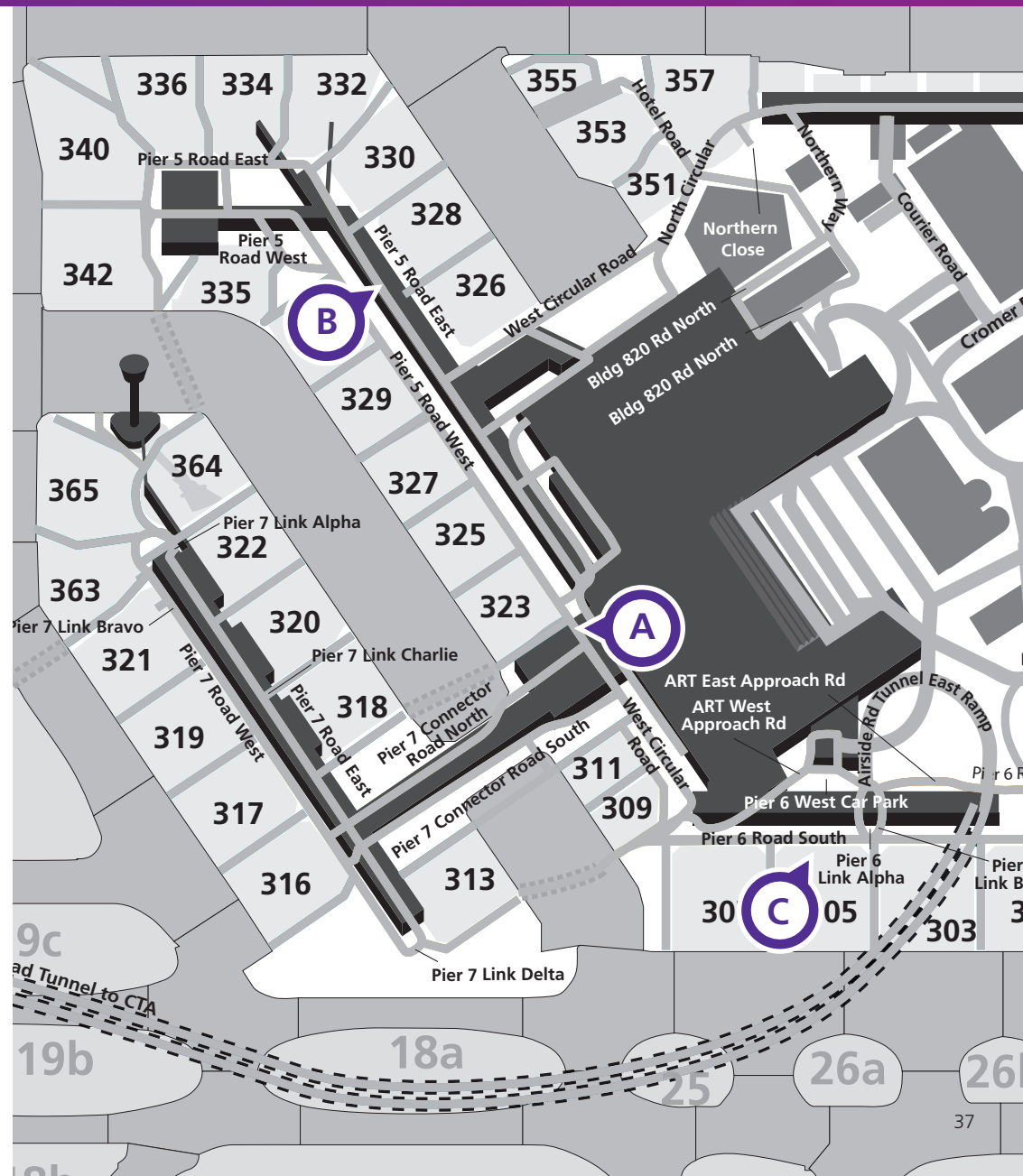
A T3 West Circular Road



B T3 Pier 5 Road East



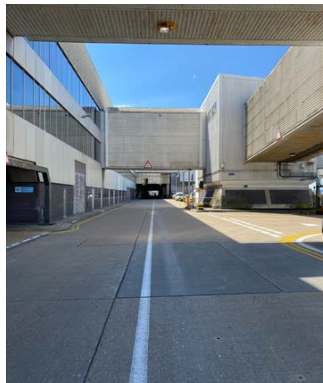
C T3 Pier 6 Road South



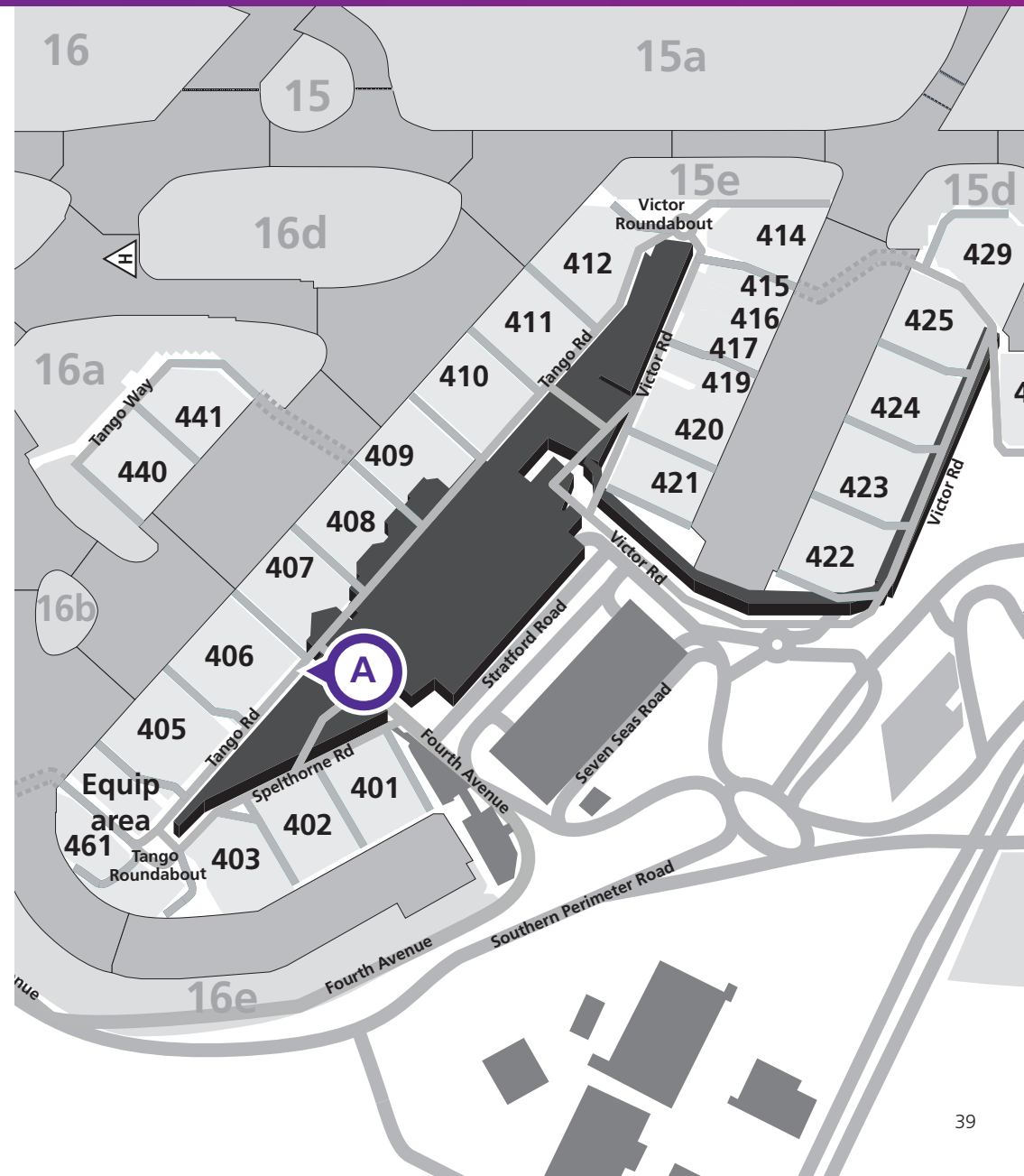
Vehicle Collisions Hotspots: Terminal 4

(Jan 2023-Jan 2024)

These are the hotspot areas for road traffic collisions at Terminal 4. Please ensure you're vigilant at all times.



A T4 Tango Road



Vehicle Collisions Hotspots: Terminal 5

(Jan 2023-Jan 2024)

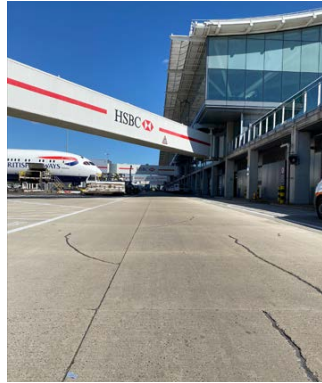
These are the hotspot areas for road traffic collisions at Terminal 5. Please ensure you're vigilant at all times.



A T5 Terminal Approach Road North



B T5 Windsor Road



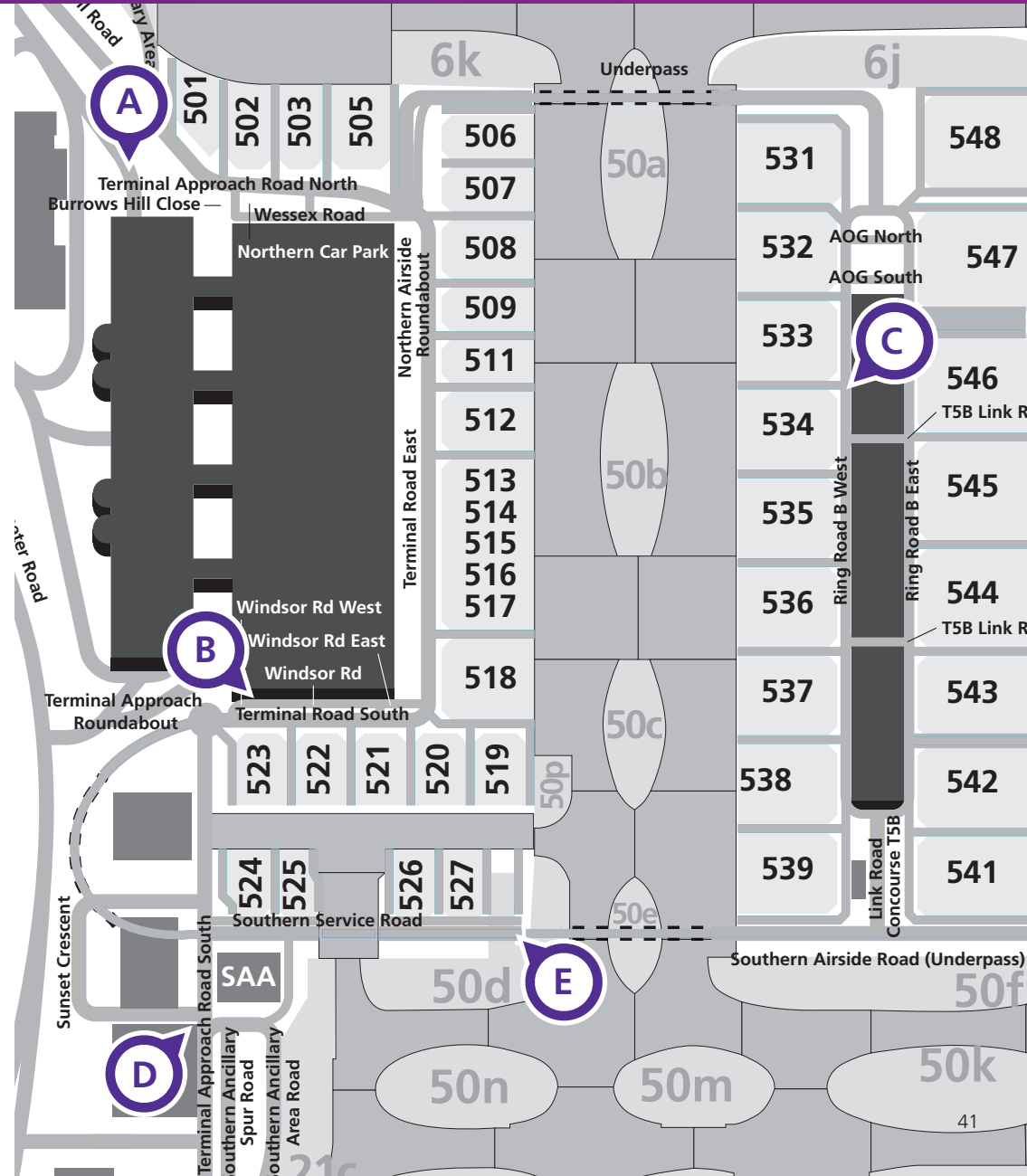
C T5 Ring Road B West



D T5 Terminal Approach Road South



E T5 Southern Service Road

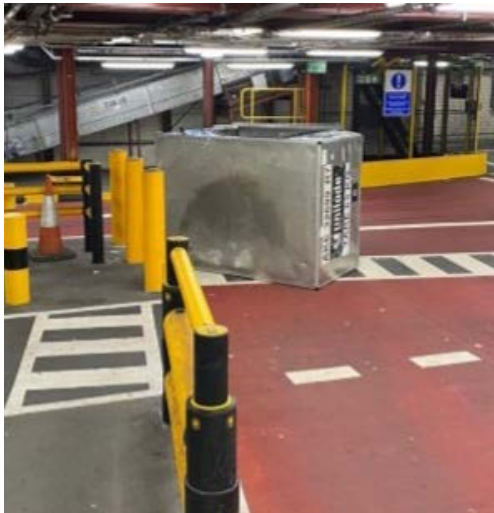


Dropped Loads

ULDs or pallets left unsecured on dollies, GSE platforms, stillages, and laterals in baggage areas, pose a significant risk of serious personal injuries, including fatalities, to personnel in close proximity. Additionally, ULDs / pallets falling onto the road network, aircraft stands, and baggage areas could lead to road traffic collision and infrastructure damage.

Therefore, it is crucial to ensure that ULDs and pallets are safely secured on equipment and are in sound condition and securely restrained before being transferred on GSE equipment and loaded into aircraft holds.

It is the driver's responsibility to ensure ULDs and pallets are adequately secured prior to moving.



In order to prevent dropped loads incidents, please follow the below steps when transporting / transferring ULDs or pallets:

- Ensure equipment, such as dollies, elevators and ULD transporters, are in good working order by conducting a thorough pre-use inspection, reporting and red-tagging any faulty equipment.
- Ensure all stops and locks are properly secured for the type of dolly you are operating through both a visual and physical inspection. Ensure that all movable parts of the device are adequately secured.
- Adjust your speed according to the road surface conditions.
- Minimise significant gaps between equipment during loading and offloading operations.
- Ensure communication and coordination are consistently maintained before moving ULDs / pallets between equipment or vehicles.
- Ensure that the rear platforms of elevators are completely lowered before transferring ULDs / pallets from dollies / cargo trucks.
- Ensure that the platforms of elevators, rear platforms, and dollies are aligned at the same heights before moving equipment across.

Dropped loads incidents must always be reported.

- Call the Emergency Services 222 (internal) or 0208 759 1212 (from any mobile phone) to report dropped ULDs / pallets happened on the ramp areas, including road network, aircraft stands, uncontrolled crossings, and ULD storage areas.
- Call Baggage Operations on 07711 146 323 (from any mobile phone) for dropped ULDs in baggage halls.

Please ensure equipment is not moved before the attendance of Heathrow Officers.



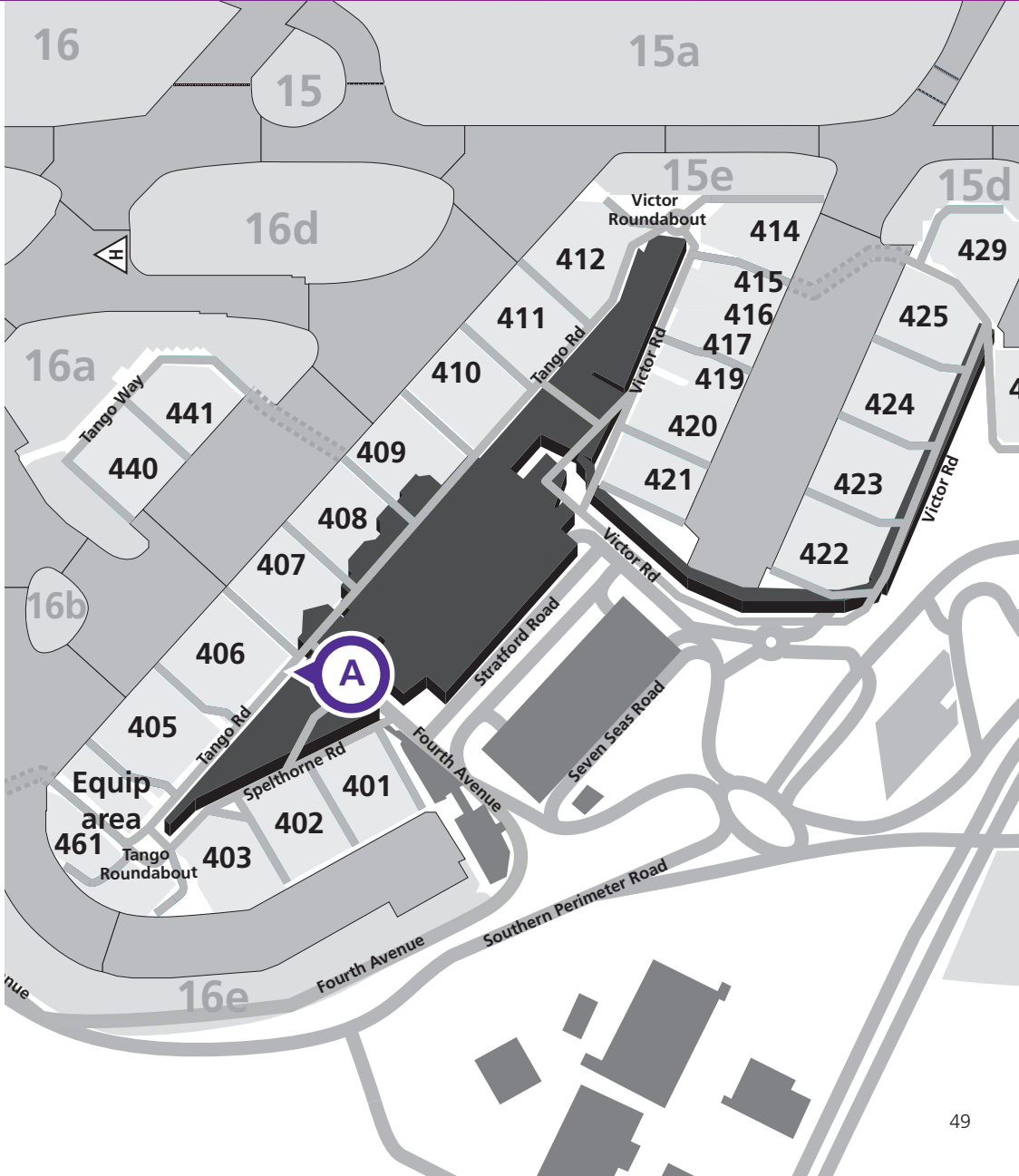
Dropped Loads Hotspots: Terminal 4

(Jan 2023-Jan 2024)

These are the hotspot areas for Dropped Loads at Terminal 4. Please ensure you're vigilant at all times.



A T4 Tango Road



Dropped Loads Hotspots: Terminal 5

(Jan 2023-Jan 2024)

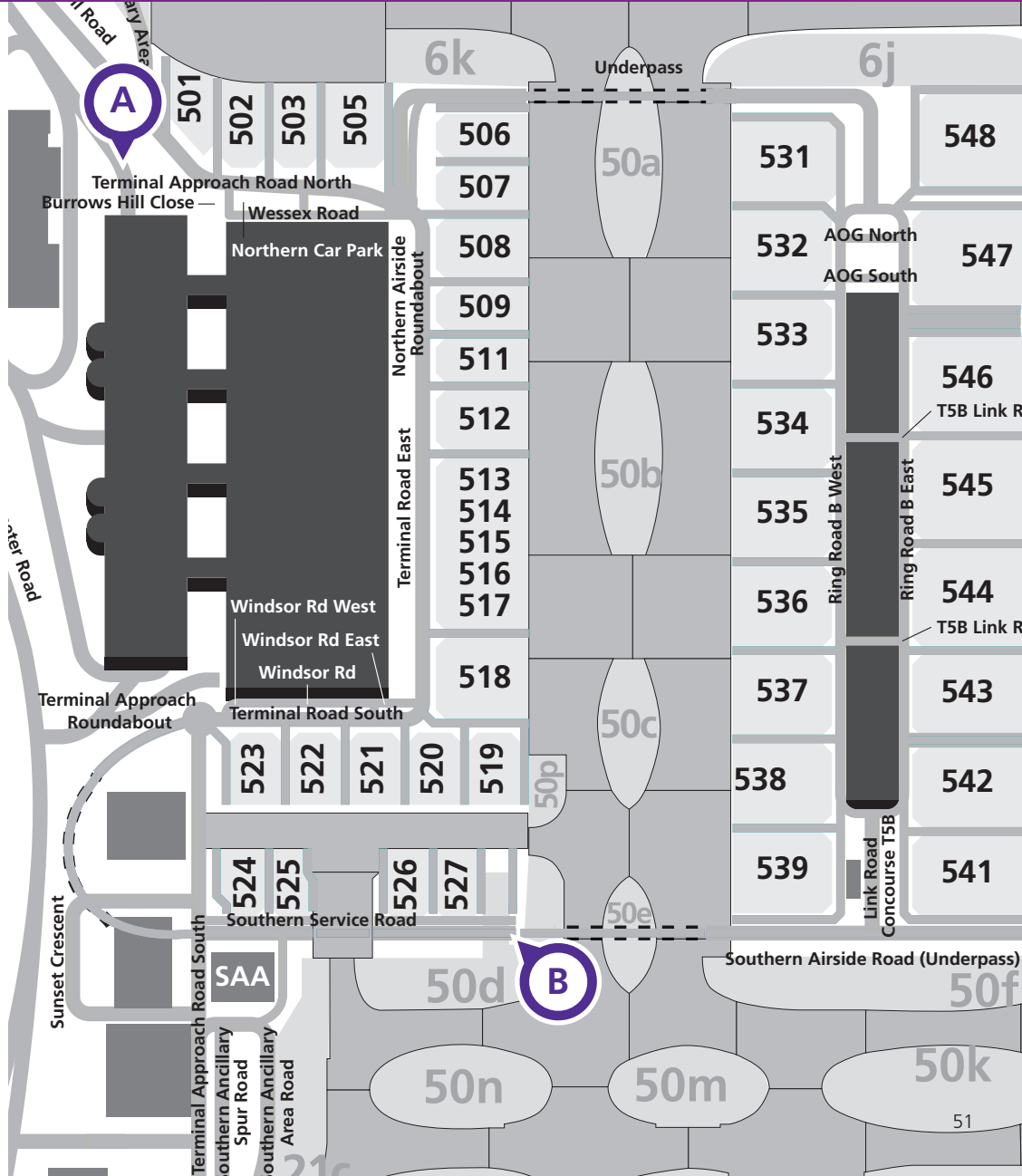
These are the hotspot areas for Dropped Loads at Terminal 5. Please ensure you're vigilant at all times.



A T5 Terminal Approach Road North



B T5 Southern Service Road



Giving way to aircraft and vehicles

Whilst driving airside, whether that be on the roads or on the manoeuvring area, it is imperative that drivers give way to all aircraft. When waiting to cross at an uncontrolled crossing, you must not only give way to aircraft, but you must also give way to vehicles travelling on the manoeuvring area. Do not drive across stands and be vigilant of aircrafts turning onto them.

If you fail to give way to an aircraft, and as a result cause a danger to that aircraft, it is a serious offence and can result in points on your licence, or even criminal prosecution under The Air Navigation Order 2016.

Uncontrolled crossings

Uncontrolled crossings are roadways that run across sections of taxiway. These crossings are marked by black and white checkerboard markings. Vehicles must remain within the markings and keep to the left of approaching vehicles.

Drivers using an uncontrolled crossing must give way at all times to aircraft / vehicles using the taxiway, and aircraft pushing back from adjacent stands. Drivers using the uncontrolled crossing must bring their vehicles to a full stop and perform a careful check of their surroundings prior to using the crossing.

Vehicles must cross behind aircraft at a distance greater than two aircraft lengths. If an aircraft is under tow, then one aircraft length distance must have passed before crossing.

All vehicles intending to use an uncontrolled crossing must have a fully serviceable obstruction light which must be switched on.



Temporary road traffic management

Airside Operations Officers may be called to direct and control road traffic for several reasons including, but not limited to, safeguarding activities, road traffic collisions, aircraft incidents on stand, fire incidents, police emergencies, terminal evacuations, tunnel incidents and infrastructure failures.

- Slow down and stop at a safe distance if you see a Heathrow airside vehicle and Airfield Operations Officers blocking the traffic.
- Slow down and pay extra care if you see traffic cones and / or barriers used to safeguard the scene as well as segregating the road / vehicles / pedestrians.
- Be extra vigilant if you see temporary road signage such as directional road signage, diversion signage, speed limit signage and vehicle matrix signs.



Pedestrian safety



1.

Ensure you use the safe walking routes and the allocated pedestrian crossings.

2.

Where a designated crossing is not available, pedestrians should use the Green Cross Code.

3.

When working on the apron, pedestrians must not walk across the double white lines unless they are under escort of Airfield Operations or involved in the pushback manoeuvring of an aircraft.

4.

Always remain vigilant of oncoming vehicles.

5.

Do not be distracted by electronic devices – keep alert at all times.

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- ASDRVE_OSI_006, v1.1**, 17th March 2017, Airside Driver Permit Requirements and Driver Training
- ASDRVE_OSI_012, v2.0**, 10th September 2021, Unit Load Device Management and Dolly / Trailer Equipment Standards
- ASDRVE_OSI_017, v2.0**, 09th January 2020, Pedestrian Walkways and Crossing Airside
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